



Mary Catherine Gibbs
mcgibbs@wiregill.com
703-836-5757

January 13, 2023

Michael Swidrak, AICP
Urban Planner III, Development Division
Department of Planning and Zoning
Alexandria, VA 22314

Re: CDSP#2022-00024 – Potomac River Generating Station Site
Comment Response Letter to Compiled City Comments for IDSP Concept #2

Dear Michael:

The Applicant, Hilco Redevelopment Partners (HRP), is in receipt of your comments on their IDSP Concept 2 submission and provides the following responses thereto along with their Completeness Submission filed contemporaneously herewith.

PLANNING & ZONING

Findings

1. The details in the IDSP shall meet the applicable design standards included in the PRGS Addendum of the Design Standards and Guidelines. Any information on final details to be provided with the block DSUPs will be reviewed at that time.

Response: Acknowledged.

2. Staff is confirming that the first Urban Design Advisory Committee (UDAC) presentation of the infrastructure DSP will take place on November 16. Materials should be submitted to City staff by November 2 for review.

Response: Acknowledged.

3. As discussed, design details and connections to the GWMP and Mount Vernon Trail (in consultation with NPS) should be shown on the Infrastructure DSP set and refined on the applicable forthcoming DSUPs.

Response: Connections to the Mount Vernon Trail and GWMP are included in this IDSP submission set however, all connections and improvements are subject to the coordination, potential changes and final approval by the National Park Service.

4. Exhibits and any additional materials should be provided to facilitate discussions with Norfolk Southern Railway regarding the public crossings of N. Fairfax and N. Royal streets.

Response: Acknowledged, Applicant has coordinated with City staff and sent those exhibits to the City to facilitate those discussions with Norfolk Southern Railway.

5. Staff will anticipate that information on sitewide remediation will be submitted with the Completeness plan materials.

Response: Applicant enrolled the site in the Voluntary Remediation Program under the Virginia Department of Environmental Quality and is being monitored by the City of Alexandria. Additional remediation activities, likely soil excavation and off-site disposal, will be conducted in coordination with demolition and redevelopment work. A meeting was held with the City Staff on December 19th to discuss these plans.

Applications

6. Continue to work with staff on filing the subdivision of the adjacent Pepco property that is to be consolidated within the CDD site.

Response: Applicant obtained Pepco approval to file the subdivision request related to the boundary line adjustment and was filed on December 12, 2022 for a March Planning Commission review and approval.

7. These applications should be submitted no later than the Completeness submission of this DSP:
 - a. A subdivision will be required to be submitted with the completeness Infrastructure DSP application to approve the subdivision of the CDD property that will occur from the dedication of the Road A right-of-way.
 - b. A street name case for Road A.

Response:

- a. **Acknowledged. The subdivision application related to the dedication of Road A is submitted with this IDSP Completeness Submission.**
- b. **Pursuant to Condition No. 157 of CDD Concept Plan #2021-00004, street name case requests for new street names within the CDD must be approved by the Planning Commission prior to the release of the first Final Site Plan for the respective CDD phase in which the public streets are located. Since Road A will be constructed with the first phase of the CDD, Applicant will file the street naming case with the first Block DSUP Completeness Submission.**

Site Plan Comments

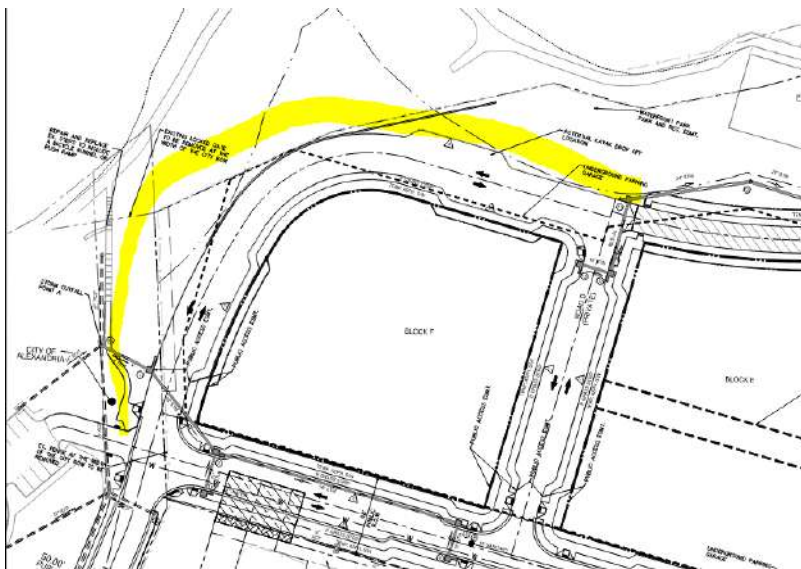
8. The applicant shall provide a common elements package as part of the IDSP plan set and for UDAC review and consideration. The package should identify proposed treatments for public and private rights-of-way (and any differences), special streetlights and furnishings. The common elements package should include:
- Sidewalk material selection (public and private streets; curbed and curbless streets)
 - Streetlights and lighting fixtures
 - Street furniture
 - Hardscape treatments around tree wells
 - Benches
 - Bus stops and shelters

Response: An initial version of a Common Elements Package (CEP) is included in this IDSP Completeness Submission. See attached Exhibit 1. Applicant will continue to detail and refine the CEP as part of the design and entitlement process for the Open Space and Block DSUPs, similar to the final site plan process for the North Potomac Yard development.

9. Ensure that the IDSP is coordinated and updated jointly with the stormwater DSP and subsequent DSUPs.

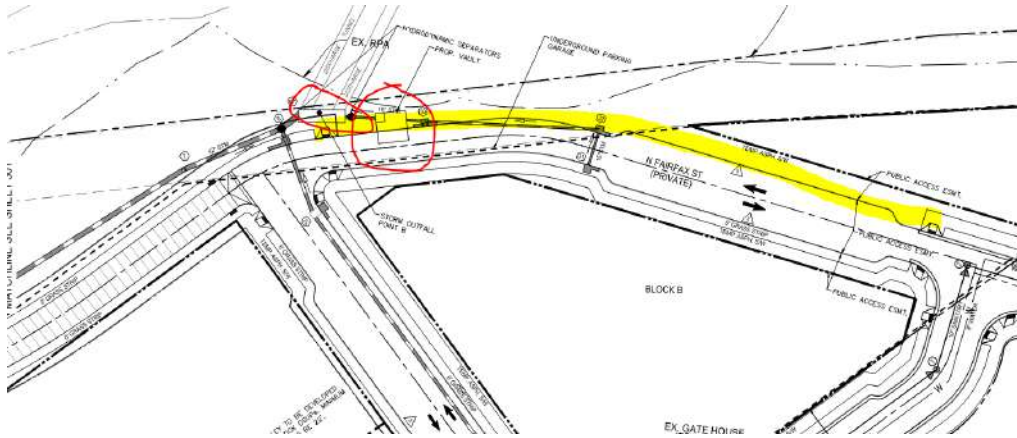
Response: The IDSP will be coordinated and updated jointly with the stormwater DSP and subsequent DSUPs.

10. Show how a sidewalk or walkway would connect pedestrians from the Slaters Lane/Road A and/or the steps to the Mount Vernon Trail to the east side of N. Fairfax Street by Block E.



Response: Pedestrians have three path alternatives to reach the east side of N Fairfax across from Block E (see image above and Sheets C301&C304). Pedestrians from Slaters Lane/Road A may use the sidewalk around Block F to reach N. Fairfax Street, which provides two ADA connections. Pedestrians may walk from the steps at the terminus of Slaters Lane that connect to the Mount Vernon Trail (MVT) along the trail and path that connect to N Fairfax across from Block E. Connections on NPS property are subject to NPS review and approval.

11. Provide a sidewalk connection along the east side of N. Fairfax Street between blocks A and B to Block C.
 - a. Staff does not support the location of hydrodynamic separators and stormwater vault in the sidewalk area and in Waterfront Park.



Response: A sidewalk connection has been added along the east side of N Fairfax St. between Blocks A and B and the vault and one hydrodynamic separator have been moved into N. Fairfax Street. (See Sheet C303). Due to the configuration of the storm system the other hydrodynamic separator must remain in the Waterfront Park and the lid placed within the paved temporary sidewalk. (See Sheet C402)

12. The retaining wall at the northeast corner of the site adjacent to the curved portion of N. Fairfax Street shall have a naturalistic design in consultation with NPS. Also provide additional details on design and if a railing will be installed on the wall.

Response: Applicant has added further details to the retaining wall design (see Sheet L004). The retaining wall is subject to NPS review, potential modification and approval.

13. Note 1 Sheet C100: note that some rights-of-way will be constructed only in final condition based on the sequencing of the Phasing Plan.

Response: Acknowledged and revised.

14. Provide more details on the curbless streets. These streets should have bollards that delineate pedestrian-only spaces.
 - a. The Road C intersections with Road A should be designed to minimize vehicular access and encourage slow speeds for accessing vehicles.

Response: Applicant has added more detail to the curbless streets (Sheets C301-304 and L003 as well as the Common Elements Package attached hereto as Exhibit 1), however, final details for curbless streets will be further designed and refined with subsequent DSUPs and Final Site Plan review.

a. Acknowledged. Road C intersections with Road A will be designed to minimize vehicular access and encourage slow speeds.

15. Provide a finalized design of Slaters Lane from the GWMP to the Mount Vernon Trail connection, as there will not be an interim streetscape condition implemented here.
 - a. Since the stairs to the Mount Vernon Trail are proposed to be maintained, show an ADA-accessible and bicycle-friendly (per Condition 73) route from the City's Slaters Lane right-of-way to the Mount Vernon Trail. The connection could be noted as requiring NPS approval.

Response: Applicant has made modifications to the design of Slaters Lane from GWMP to the Mount Vernon Trail (Sheets C301 and C302). Pursuant to Condition No. 32(d) of CDD Concept Plan # 2021-00004, "the permanent/final condition of improvements to Slaters Lane may be delayed if potential construction traffic impacts make interim conditions more appropriate subject to the determination and satisfaction of the Director of T&ES."

a. Refer to response and image to Question #10 above. Any connections on NPS property are subject to NPS review and approval.

16. With the next submission, depict and label in plan view the 20-foot setback for Blocks B, C, E and F for reference as required by the CDD approval.

Response: Acknowledged. Applicant identified and labeled the 20-foot setback for Blocks B, C, E, & F in Sheets C305 and 306. Final setbacks will be determined as part of each individual Block DSUP pursuant to CDD Condition # 57.

17. Explore if on-street parking is feasible for one side of N. Fairfax Street between Third Street and Road A. The roadway width between each curb appears to be about 33-feet wide and could potentially accommodate parking.

Response: Acknowledged. On-street parking on the east side of N. Fairfax Street between Road A and Third Street is shown on Sheet C303.

18. The buildable footprint of Block E as shown encroaches into the sidewalk and streetscape of N. Fairfax Street. This footprint may need minor adjustment to properly relate to the future streetscape. Any building area footprints are considered illustrative for the IDSP approval.

Response: Acknowledged. The final layout of the building footprint and streetscape will be provided with Block E DSUP.

19. Provide a more detailed grading and topography exhibit between the site and Bashford Lane to ensure the infrastructure plan is designed (alignment and grading, etc.) to not preclude a future street connection to N. Pitt Street as envisioned by the Master Plan and CDD approval.

Response: Acknowledged. Additional details for the potential Pitt St. connection on PRGS property are shown on Sheet C503. Applicant does not own the portion of property that connects N Pitt at Bashford, a survey of this property could not be performed and the topography of this property is not included.

20. Provide in greater detail the potential N. Pitt Street connection, including with a 66-foot right-of-way, roadway and sidewalks. Label on plan set as potential right-of-way.

Response: Applicant has provided additional detail for a potential N. Pitt St connection on PRGS property on Sheet C304, however, PRGS does not own the auto body shop at N. Pitt and Bashford and a survey could not be performed. See also response to Question 19.

21. Underground garage comments:

- a. Provide more details on the course and boundaries of the underground garage, specifically near Block E. Staff generally does not support the location of the underground garage within the Waterfront Park space.
- b. An underground parking garage layout exhibit should be provided to demonstrate the overall plan and extent of the garage with approximate entrance locations.

Response: A conceptual underground parking garage layout is provided on Exhibit 2 of this comment response letter and illustrates the proposed extent and shape of the garage. The garage design depends on the square footage, land use program, and structural grid design of the buildings above the garage and will evolve in parallel

with the block and building designs. Courses and boundaries will be provided with Block DSUPs.

22. Details and plan sheets should be provided for any streets that include tree wells alternating with on-street parking spaces.

Response: Acknowledged. The IDSP addresses temporary BMP measures which don't propose alternating tree wells with on-street parking spaces. Tree wells alternating with on-street parking spaces may be proposed and addressed in future Block DSUPs.

23. Show the potential locations of the Capital Bikeshare stations.

Response: Potential locations of the Capital Bikeshare stations are shown on the Site Plan (Sheets C301 and C303).

24. Coordinate notes across plans for the alleys on Block C and E (i.e. remove 'possible' from 'possible through block alley' on Sheet A300).

Response: Acknowledged, this has been updated.

25. Woonerf comments:

- a. Woonerf design should be a final design since there is no temporary Woonerf design based on the approved Phasing Plan.
- b. Note location of bollards (item 2).
- c. Provide a mid-block crossing adjacent to the Central/Waterfront Plaza that connects to the Pump House.
- d. Provide more details of the final design of the sidewalk adjacent to the Woonerf.
- e. Provide more general details about the pavers (size, material, if permeable).
- f. Show a closer detail on materials and design of the transition slope and crosswalks.

Response:

- a. Conceptual Woonerf design is included in the Common Elements Package submitted as Exhibit 1. The final condition of the Woonerf is required to be constructed by the completion of Phase 2 pursuant to Condition 32(a). Final design of the Woonerf will be coordinated with the design and implementation of the Waterfront Park, Block D and the Waterfront Plaza. Proposed details for the Woonerf design, subject to refinement as part of the Waterfront Park, Block D and the Waterfront Plaza DSUPs are included in Sheets L003.

- b. Bollards are not proposed as part of the Woonerf design. Alternative programmatic protection elements will be included in the final design of the Woonerf, please refer to response to 25(a) above.**
 - c. A mid-block crossing is shown on Sheet L003 and Preliminary Pavement Marking and Signage Plan, Sheets C307-C309.**
 - d. Refer to the Common Elements Package. Further details will be provided in future Waterfront Park, Block D and the Waterfront Plaza DSUPs.**
 - e. Refer to the Common Elements Package. Further details will be provided in future Waterfront Park, Block D and the Waterfront Plaza DSUPs.**
 - f. Refer to the Common Elements Package. Further details will be provided in future Waterfront Park, Block D and the Waterfront Plaza DSUPs.**
26. Provide a separate plan sheet that delineates each area with a future dedication (public right-of-way), public access easement (private rights-of-way and applicable open space) and park and recreation easements for reference.
- a. Confirm that the ownership and maintenance responsibilities (repairs, paving, snow removal, etc.) of the private rights-of-way will be held by a master association or coordinated entity.

Response: See Sheets C102, C103, and C104 for identification of future public road and public access easements on private roads and open spaces.

- a. Private rights-of-way and maintenance responsibilities will be held by a successor entity and/or master association of Applicant.**
27. Include the private and public right-of-way extents on the plan sheets. For instance, show the 50-foot minimum right-of-way width for N. Fairfax Street north of the Woonerf per Condition 76.

Response: Acknowledged and added to Sheets C102 and C306.

28. The water line shown through Waterfront Park should be relocated or moved closer to the right-of-way to minimize the impact to the park per Condition 97 of the CDD Concept Plan.

Response: Acknowledged. Option 2 utility plan has been removed from the plan set and the waterline is located in Road A.

29. Confirm if the kayak drop-off area will remain in same location if the launch area changes.

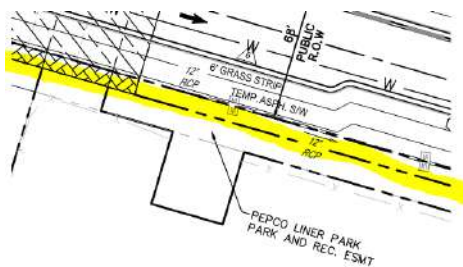
Response: The proposed kayak launch is located at the most favorable place from a topographical, marine engineering and maintenance standpoint. If a change is necessitated by NPS, the location of the kayak drop-off area may need to be reconsidered.

30. Show the final roadway and streetscape improvements for the N. Royal and N. Fairfax street extensions into the site from Third Street and Bashford Lane to the southern property line per Condition 30B (Phase One).

Response: Refer to Sheets C303 and C304.

31. Sheets C301 and C306:

- a. The plans appear to label a 68-foot right-of-way instead of a 66-foot right-of-way (and measure to be 68 feet wide). Only a 66-foot right-of-way is required.
- b. the arrow depicting the Pepco Liner park and recreation easement appears to point at property that is to remain Pepco's. More clearly delineate the CDD site and Pepco property (1300 and 1400 N. Royal Street) properties.



Response:

- a. The cross section on Sheet C809 is updated to show a 68-foot wide right-of-way, required to accommodate the gutter plans.
- b. Acknowledged and corrected, see Sheet C301.

32. Provide more specific building envelope and site details for The Muse property at 1201 N. Royal Street to better depict its adjacency to the future N. Fairfax Street right-of-way.

Response: Acknowledged and added, see Sheet C303.

33. Open Space Plan (Sheet A300) comments:

- a. Confirm the width of OS-9 and OS-10 based on a 66-foot-width of Road A (shown on the plan set as 68-feet wide). Regardless of the OS-9 and OS-10 width, the open spaces should be designed creatively as a linear open space that relates to the adjacent sidewalk and Rail Corridor Park to the south.

- b. The “pedestrian zone” or main walkway area of sidewalks within private rights-of-way do not count toward open space requirements. However adjacent amenity or frontage zones over 8 feet may be able to count toward open space. Please see the image on the next page.
- c. Provide a proposed outdoor dining exhibit or shade the areas on the open space exhibit (to be updated as needed with subsequent submissions).

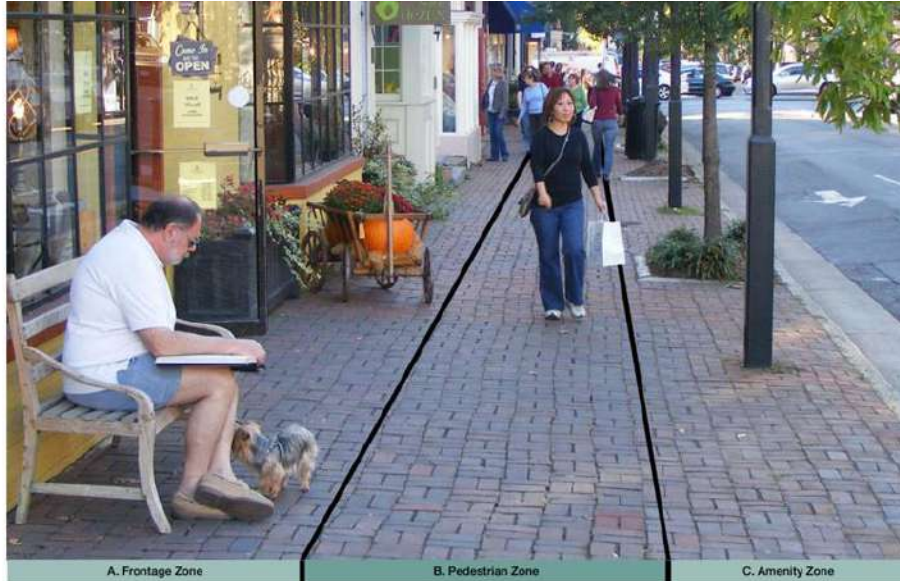


Figure 1: Delineation of sidewalk zones per the City Complete Streets Guidelines (page 3-3).

Response:

- a. **Acknowledged. Please refer to response to Question 31(a) regarding street width.**
- b. **Per the Zoning Ordinance sidewalks adjacent to open spaces that meet the definition for open space are to be included in the calculations for open space.**
- c. **See Sheet A300.**

34. The Phasing Plan (Sheet A303) should reflect that the Pump House will be developed by the end of Phase Two, per Condition 31a of the CDD Concept Plan.

Response: Condition 32c provides that the Waterfront Park will be completed in Phase 2, not that the Pump House will be developed in Phase 2. The second sentence of 32c refers to modified plans for the Waterfront Park, exclusive of the Pump House. The development of Pump House is not associated with any phase of development and is subject to coordination with NPS.

Landscape Guidelines

35. Proposed tree, shrub and groundcover plantings in the right-of-way will be determined with each forthcoming DSUP.

Response: Acknowledged.

36. Provide the following information with the tree schedule:
- Note if the trees are within the CDD boundary, off-site, or off-site but potentially impacted by off-site work.
 - Note if each tree is to be removed or preserved.
 - Provide the caliper and tree health information.
 - If disturbance occurs within a Resource Protection Area (RPA), trees to be removed within the disturbed portion of the RPA buffer area shall be inventoried at ½" diameter and greater, and all shrubs to be removed within the disturbed portion of the RPA buffer shall be inventoried at 3 feet in height and greater.
 - Critical root zone extent for any trees that could be impacted by construction related to the infrastructure DSP.
 - Identify any invasive trees. A landscape management plan for the removal of invasive species is required per Condition 130 of the CDD Concept Plan.
 - Additional information for any trees that could be impacted in wooded areas adjacent to or part of NPS property may be requested per page 11 of the Landscape Guidelines.

Response:

- Refer to Sheets C205-C208.**
- Refer to Sheets C205-C208 and Sheets C210-C215 for the Tree Preservation Schedule.**
- Refer to Sheets C210-C215 for caliper and health information for the onsite and offsite trees.**
- Acknowledged. Refer to Sheet C208 for surveyed location of trees at ½" diameter and greater and shrubs 3 feet in height and greater. See Sheet C214, Tree Preservation Schedule, for a list of these species within the RPA.**
- Refer to Sheets C205-C209.**
- Refer to Sheets C210-C215. A Landscape Management Plan is provided on Sheet C216, including the location of invasive species. Tree Preservation is shown on Sheets C210 to C212.**
- Acknowledged.**

37. A tree and vegetation protection plan will be required for any trees on or off-site if applicable per pages 14-16 of the Landscape Guidelines.

Response: See Sheet C205-C209 with notes and details on Sheet C215.

Utilities

38. If the two options for utility placement are still needed, provide a narrative explaining differences between Site Plan Options 1 and 2.

Response: Only one option is proposed in this Completeness Submission of the IDSP. Option two was removed from the plan set.

39. Provide existing (to remain) and proposed dry utilities on the plan set. Provide the locations of streetlights (for temporary streetscape).

Response: The existing utilities and proposed dry utilities are shown on Sheets C400-C403.

40. Explore feasibility for 5G or other smart technology capabilities within streetlights.

Response: Acknowledged.

41. With the next submission, depict location of duct bank conduit to be used for fiber and other telecommunications equipment, in coordination with staff from ITS and T&ES.

Response: Refer to Sheets C400-C403.

Sustainability

42. Staff will be providing additional sustainability related comments to the applicant with the CSS comments in mid-November, in addition to feedback to be provided from the EPC, Planning Commission and City Council in the upcoming work sessions.

Response: Acknowledged, Applicant received additional comments from the City on November 21, 2022.

43. Include any infrastructure required for sustainable measures identified in the CSS, including for on-site energy generation. Provide a written narrative about proposed strategies to increase on-site generation beyond 3 percent, per the CDD condition 139b.

Response: Acknowledged. Exhibit 3 has been added to this response letter, which identifies potential locations for on-site renewable energy generation at PRGS, independent of per building on-site generation. The CSS addresses on-site energy generation in narrative form. On-site energy generation will be refined with individual Block DSUPs.

44. Include green infrastructure in the rights-of-way and adjacent public spaces as instructed by the sustainability related CDD conditions of approval.

- a. Provide information and details on any interim bioretention planters to be installed.

Response: Acknowledged. Interim bioretention planters are located along the roads and a grass swell along the east side of the Woonerf see Sheets C604-C607. Details for permanent bioretention planters and swales are on Sheet C603. Final locations of permanent bioretention planters will be determined as part of each Block DSUP.

45. Provide analysis how the infrastructure plan will target the embodied carbon reduction as identified in the CDD conditions of approval.

Response: Applicant acknowledges embodied carbon is a factor in infrastructure decisions and has addressed the embodied carbon target in the CSS. Note the CDD conditions specific to embodied carbon refer only to new buildings constructed on the site.

Future Conditions

46. The underground garage structure will be constructed to permit a minimum planting depth for shade and street trees above.

Response: Acknowledged. See sections added to Sheet A400.

47. A master association will be responsible for ownership and maintenance of privately owned and publicly accessible rights-of-way and open spaces.

Response: Acknowledged.

48. All transformers shall be located below grade and out of public rights-of-way and all open spaces.

Response: Acknowledged, unless otherwise approved by Dominion Energy in above ground transformer vaults within buildings.

TRANSPORTATION AND ENVIRONMENTAL SERVICES (T&ES)

Findings

1. OEQ and PWS has no comments.

Response: Acknowledged.

2. Resource recovery, Dash, and VAWC provided no comments.

Response: Acknowledged.

Comments

1. The next submission of this plan must be prepared per the requirements of Development Preliminary Site Plan Checklist and Memorandums to Industry 02-09 dated December 3, 2009 on Design Guidelines and City Standard Notes. (D-ROW)

Response: Additional notes appropriate for a preliminary plan have been added to Sheet C100. Other notes and details specific to final site plan conditions and/or construction plans referred to in the Memo to Industry will be submitted with Final Site Plan Submissions.

2. Provide preliminary computations on generation of pre and post development runoff from the site using the rainfall depths for the City of Alexandria as: 1 Yr 24 hour = 2.70, 2 Yr 24 hour = 3.20 and 10 Yr 24 hour = 5.20 inches per NRCS (formerly SCS) TR-55 method. The overland relief shall be provided for 100 Yr 24 hour = 8.2 inches of rainfall depth in the final site plan. (D-ROW)

Response: Refer to Sheets C604-C609.

3. Provide details and supporting calculations for storm water management design that include adequate outfall analysis. Include a complete drainage area map, in acres, including all off-site drainage areas in their entirety, supported by an outlined contour map. (D-ROW)

Response: Refer to Sheets C604-C609.

4. Call out on the plan that all existing overhead utilities and utility poles, specifically along Bashford Lane, shall be relocated underground. (D-ROW)

Response: One existing utility pole will be undergrounded on Bashford Lane, a note has been added to Sheet C402.

5. Lighting shall be provided along all street frontages per City Standards regarding fixture type and illumination. (D-ROW)

Response: Acknowledged, with the exception if other fixtures are approved for private streets as part of the Common Elements Package.

6. Adjust the angle of the sanitary sewer bend at structure 4 to ensure the angle is greater than 90 degrees. (DROW)

Response: This comment is applicable only to Utility Option 2, which has been removed from this submission.

7. The signalized intersection at N. Royal Street/Road B will require further analysis and discussion with staff as it pertains to the operation. If a signal is appropriate, consider the phase in which the signal will be installed. (Transportation Engineering)

Response: Acknowledged. Applicant is coordinating with City staff about signalization at intersections within/adjacent to the site. Based on a sensitivity analysis submitted to the City, it is recommended that the Road A & N Royal Street and Bashford Lane & N Royal Street intersections operate under all-way stop control with the opening of the streets. Phasing and operations of the signals will be studied and coordinated further with City staff as part of subsequent DSUPs.

8. Slaters Lane extension does not appear to include a curb and gutter. However, there is no indication that it ramps up to sidewalk level and there is a ramp at the N. Fairfax Street Woonerf area. Please verify and/or modify. (Transportation Engineering)

Response: The road wrapping Block F (North Fairfax Street) is a private street and does not require a gutter. A curb is shown on the previously submitted cross sections and again in this submission on road cross sections G and R on Sheets C808 and C809.

9. Road D does not appear to have curb and gutter. Please verify and/or modify. (Transportation Engineering)

Response: Road D is a private road and does not require a gutter. A curb is shown on the previously submitted cross sections for Road D and is shown again on road cross section E on Sheet C808.

10. All bike lanes leading into an intersection that continues into a sharrow should include a bike box at the stop bar. (Transportation Engineering).

Response: Applicant is coordinating with City staff on intersection controls at clustered intersections within/adjacent to the site and feasibility of bicycle boxes with signal design and operations.

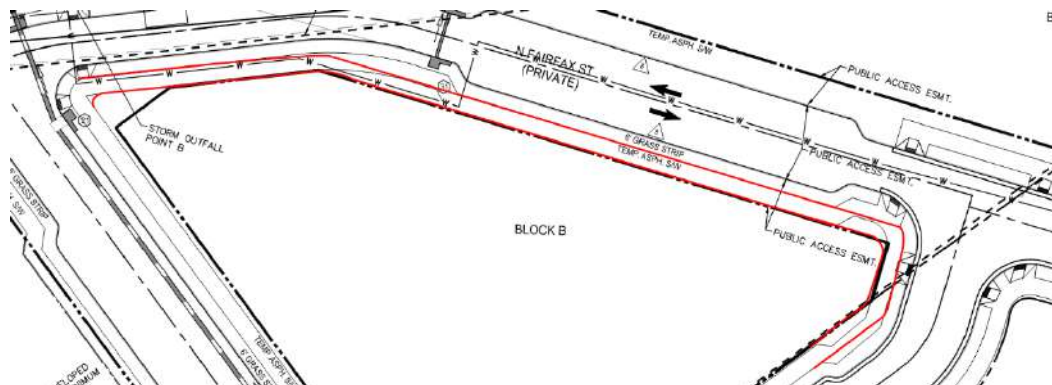
11. Show the bike lanes with curb and gutters on the street cross sections for roads on the bike route. (Transportation Planning)

Response: Bike lanes are shown on the Pavement Marking and Signage Plan (Sheets C307-C309). Bicycle lanes have been provided that meet City standards.

12. The garage footprint is unusual. Provide an exhibit that shows details of the below-grade parking garage. (Transportation Planning)

Response: The conceptual layout of the garage is shown on Exhibit 2 to further illustrate the rationale of the shape and extent of the garage, in addition to the general outline shown on Sheet C300. Details of the garage layout depend on the design and program of the vertical structures above including land use program, total square footage, and location of structural column grids. Further details will be provided with each Block DSUP.

13. Sidewalks on site should continue straight to minimize curves and provide a more direct path for pedestrians, like shown below. (Transportation Planning)



Response: Sidewalks have been revised, see Sheets C301-C304. Further refinement of sidewalk details within blocks will be included in individual Block DSUPs.

14. Sheet C201. The existing conditions are missing second storm line and outfall on slaters lane side. It is possible that this area is also contaminated and previous site leaks have occurred through this pipe. In addition, a SWM easement is shown but there is no line to dictate the location. (SWM)

Response: The existing 21” storm outfall in Slaters Lane is shown on Sheet C201.

15. Sheet C303. No hydrodynamic or detention may be located in the roadway. Hydrodynamics may not be used unless all GI has proven to be infeasible. (SWM)

Response: The location of one hydrodynamic separator and proposed vault is located in a private street. See Applicant response to P&Z Comment 11 above for more detail.

16. This plan shows no stormwater compliance. The Phase, coordinated with the Stormwater Master Plan must show it meets the minimum stormwater requirements for approval with the next submission (SWM). In addition, green infrastructure must be used to the maximum extent possible, MTD may not be used unless it is proven GI is not feasible. All public roads must be treated per the City’s Green Streets and Sidewalks Guidelines. Provide a “For information only” VRRM spreadsheet demonstrating this Phase’s compliance with stormwater requirements. (SWM)

Response: See Sheets C604-C609 for interim stormwater compliance with this IDSP.

17. Comment carried until satisfied. Per the CDD conditions, A stormwater management master plan must be submitted and approved prior to approval of the preliminary plan. (SWM)

Response: Acknowledged. A stormwater management master plan was submitted on October 7, 2022. Comments were received on November 10, 2022 and the comments addressed on the plan resubmitted on December 16, 2022.

18. Comment carried until satisfied. A Major WQIA must be submitted and approved prior to approval of the preliminary plan. Refer to Chapter 13 of the Zoning ordinance for requirements. Although final mitigation may be provided with the Waterfront plan, the WQIA must be provided with this plan. (SWM)

Response: Acknowledged, see Sheet C610.

19. Comment carried until satisfied. Per the CDD conditions, an assessment of the stormwater outfall condition must be performed by the applicant and submitted to the City for review. This must occur prior to preliminary plan approval. Ownership and adequate maintenance access must be coordinated and provided by the applicant to allow the City access to maintain the portion of the outfall located on National Park Service property in perpetuity to the satisfaction of the Director of T&ES. (DROW, SWM)

Response: Acknowledged.

20. The plan now includes the use of a different outfall into the Potomac in addition to the previously discussed tunnel outfall. This outfall must meet the same conditions as the other outfall. An assessment of the stormwater outfall condition must be performed by the applicant and submitted to the City for review. This must occur prior to preliminary plan approval. Ownership and adequate maintenance access must be coordinated and provided by the applicant to allow the City access to maintain the portion of the outfall located on National Park Service property in perpetuity to the satisfaction of the Director of T&ES. (DROW, SWM)

Response: Acknowledged.

21. The applicant shall complete a sanitary sewer adequate outfall analysis (AOA) in accordance with Memo to Industry 06-14 with submission of the preliminary site plan. (Sanitary)

Response: See Sheets C700-C703 for sanitary plans and computations.

22. Applicant to include the following note on the DSP plans and the plans issued for construction:
- a. Contractor shall ensure all discharges are in accordance with City of Alexandria Code Title 5, Chapter 6, Article B. (AlexRenew)
 - b. Dewatering and other construction related discharge limits to the sewer system are regulated by AlexRenew Pretreatment. Contractor is required to contact AlexRenew's Pretreatment Coordinator at 703-721-3500 x2020. (AlexRenew)

Response: See notes 11 and 12 on Sheet C100.

23. Applicant shall provide calculations for post-development sewer flows from the site per City of Alexandria Memo to Industry 06-14. If the planned flows exceed 10,000 GPD. Applicant shall submit a sanitary sewer outfall analysis.

Response: See Sheets C700-C703 for sanitary plans and computations.

24. Survey section can approve this Concept 2 routing. The following comments should be addressed on the preliminary plan or next Concept routing, whichever comes first. (Survey)

Response: Acknowledged.

25. Some of the annotation on the ‘overall’ existing condition plan does not match exactly that shown on the individual existing condition sheets. For example, different bearings are shown for the 80.09 and the 54.76 lines. Please make it consistent between all sheets. (Survey)

Response: Acknowledged and revised.

26. Repeat comment: On all layout sheets (such as C200 and C203) please depict the proposed ROW lines for N Royal and N Fairfax Street. Note: the proposed ROW on south side of N Royal St extension is still missing, for instance. (Survey)

Response: Acknowledged and revised.

RP&CA

1. In response to applicant comment response regarding the park planning process, staff confirms that the park planning process for Waterfront Park and Rail Corridor Park will be a standard City park planning process (as outlined in the Concept 1 comment letter).

Response: Prior to submission of the Concept II IDSP, Applicant met with RPCA staff who clarified that the comment regarding community outreach for Waterfront and Rail Corridor Open Space DSUPs is specific to meetings with City staff stakeholder groups and that public outreach will occur as part of the Open Space DSUPs design development, not that a formal City public park process will occur in addition or as part of the DSUP process.

2. Sheet L001 and A300, Label the Abingdon connector in Rail Corridor Park. Indicate the approximate square footage of open space that would be impacted if the road were built.

Response: Sheets L001 and A300 have been revised.

3. Sheet L001 show the trails and sidewalks from sheet A300.

Response: Sheet L001 has been revised.

4. Sheet C100, update the open space acreage to reflect the acreage totals on Sheet A300.

Response: Sheet A300 has been updated.

5. Sheet C200A, as required by the Alexandria Landscape Guidelines, the existing tree survey shall be prepared by an Arborist professionally certified by the International Society of Arboriculture (ISA) or an Arborist professionally registered with the American Society for Consulting Arborists (ASCA) to inventory and identify trees on the project site and within 15 feet of the project limits. The condition of each tree, including species, must be included, using the methods outlined in the most recent version (10th edition) of the Council of Tree and Landscape Appraisers (CTLA) Guide for Plant Appraisal.

Response: Sheet C200A has been replaced with Sheets C205-CC216, which were prepared by a certified arborist.

6. Finding: The open space acreage within the project limits has decreased from the approved CDD, in particular the Pepco liner. Although the Pepco liner has been reduced to accommodate the right of way, staff encourages the applicant to find creative ways to screen the Pepco plant, provide dog relief and/or exercise areas to improve the experience along the plant.

Response: The proposed total open space exceeds the minimum amount of open space required by Condition No. 95 of CDD Concept Plan # 2021-00004, which listed approximate acreage to be refined in later Block and Open Space DSUPs.

ARCHAEOLOGY

Archaeological Findings

- F-1 The Bellevue Plantation was established on the subject property shortly after the American Revolution. By 1801 William Hodgson and his wife Portia Lee began leasing the 17-acre plantation that bordered the Potomac River. At that time the main house was described as a “50 ft. long & 28 ft. wide, 1 story high with a Dutch roof and was constructed of wood.” The house had a 20 ft. by 23 ft. brick cellar. Nearby stood a wood frame 28 ft. by 18 ft. kitchen and the yard was populated with a stable, smokehouse, and dairy. The estate was valued at \$4,000 in 1795, one of the more valuable plantation properties in the area. Hodgson was a local merchant and regularly sold goods from his store on Prince Street. By 1820 Bellevue Plantation was put up for sale. Later in the 1840s John Slater acquired Bellevue, built greenhouses, and established a floral business. Slater had learned the trade from William Yeates, a prominent local horticulturalist. During the Civil War there are at least three small “farms” (possibly greenhouses) depicted on the property, each fenced and planted in orchards or other formal plantings, such as might have been part of Slater’s floral business. The property remained in the Yeates family into the twentieth century and continued to operate as a productive farm.

Response: Acknowledged.

- F-2 If this project is a federal undertaking or involves the use of any federal funding, the applicant shall comply with federal preservation laws, in particular Section 106 of the National Historic Preservation Act of 1966. The applicant will coordinate with the Virginia Department of Historic Resources and the federal agency involved in the project, as well as with Alexandria Archaeology.

Response: Acknowledged.

Open Space and Landscaping

1. Hire a professional consultant to work with staff and the landscape designers to incorporate and interpret elements of the historical character and archaeological findings into the design of the open space and to prepare interpretive elements, which shall be erected as part of the development project. The site plan shall indicate themes and locations of interpretive elements. Prior to release of the final site plan, the consultant shall provide text and graphics for the signage subject to approval by the Office of Historic Alexandria/Alexandria Archaeology and the Directors of P&Z and/or RP&CA.* (Arch)(P&Z)(RP&CA)

Response: Acknowledged. Applicant is working with EHT Tracerics to assist in the development of the overall Historic Interpretation Plan that will inform the design of the open space and prepare for interpretive elements in the development. This will be submitted before or contemporaneously with the first preliminary DSUP plan submission.

Archaeology Conditions

1. The applicant hired an archaeological consultant who completed a Documentary Study and an Archaeological Evaluation. Based on the degree of prior disturbances within the study area, the consultant recommended that no further archeological work is necessary for the study area. Alexandria Archaeology concurs with this recommendation.

Response: Acknowledged.

2. Call Alexandria Archaeology immediately (703-746-4399) if any buried structural remains (wall foundations, wells, privies, cisterns, etc.) or concentrations of artifacts are discovered during development. Work must cease in the area of the discovery until a City archaeologist comes to the site and records the finds. If significant resources are discovered, the consultant shall complete a Resource Management Plan, as outlined in the City of Alexandria Archaeological Standards. Preservation measures presented in the

Resource Management Plan, as approved by the City Archaeologist, will be implemented. (Archaeology). The language noted above shall be included on all final site plan sheets involving any ground disturbing activities. (Archaeology)

Response: Acknowledged.

3. The applicant shall not allow any metal detection and/or artifact collection to be conducted on the property, unless authorized by Alexandria Archaeology. Failure to comply shall result in project delays. The language noted above shall be included on all final site plan sheets involving any ground disturbing activities. (Archaeology)

Response: Acknowledged.

4. Certificates of Occupancy shall not be issued for this property until interpretive elements have been constructed, interpretive markers have been erected, and the final archaeological report has been received and approved by the City Archaeologist.*** (Archaeology)

Response: Acknowledged. As previously stated in the comment responses submitted with IDSP Concept II submission, Applicant will implement the Historical Interpretive Plan with each Block and Open Space DSUP.

Code

- C-1 All required archaeological preservation measures shall be completed in compliance with Section 11-411 of the Zoning Ordinance.

Response: Acknowledged.

FIRE DEPARTMENT

Fire Department comments will come with the next submission.

Response: Acknowledged. Note there are no buildings for review in IDSP.

We continue to look forward to working with you towards the approval of this Infrastructure DSP.

Sincerely,

A handwritten signature in cursive script, reading "Mary Catherine Gibbs".

Mary Catherine Gibbs

Attachments:

- Exhibit 1 - Common Elements Package (CEP).
- Exhibit 2 - Conceptual Underground Parking Garage Layout.
- Exhibit 3 - Potential Locations On-site Renewable Energy Generation

cc: Michelle Beaman Chang, HRP, VP, Mixed-Use Development

The background image shows the Potomac River Generating Station, a large industrial facility with complex metal structures and pipes. In the foreground, a wooden walkway with a rope railing leads towards the station. To the left of the walkway is a wall covered in a colorful mural of fish and sailboats. A large, curved metal pipe is visible on the station's facade.

OJB

POTOMAC RIVER GENERATING STATION

EXHIBIT #1: COMMON ELEMENTS DESIGN GUIDELINES

JANUARY 13, 2023

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PREPARED BY

OJB LANDSCAPE ARCHITECTURE
One Bowdoin Square
Suite 801
Boston, Massachusetts 02114
617.426.1300

HILCO REDEVELOPMENT PARTNERS
1199 N Fairfax St.,
Suite 808
Alexandria, VA 22314
312.796.6564

VERSION	ISSUE DATE
CONCEPT 3 SUBMISSION	01/13/2023

General Streetscapes

STREETSCAPES

Typologies



SPINE ROAD
Mixed-Use Boulevard
(Public Street)



WATER CONNECTORS
Neighborhood Connector
(Private Street)



FAIRFAX EXTENTION
Parkways
(Private Street)



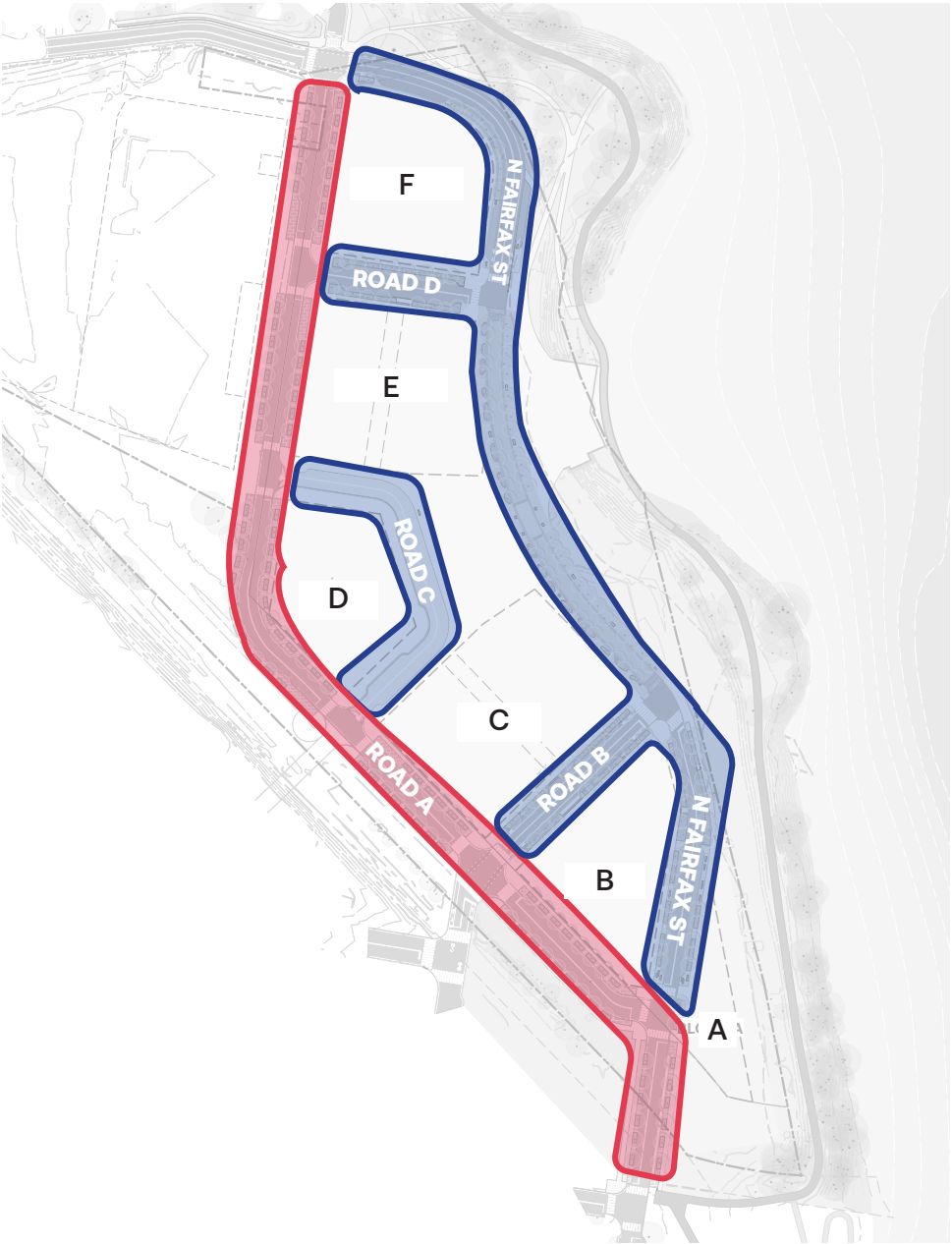
SPECIAL RETAIL & DROP-OFF
Shared Street
(Private Street)



WOONERF
Shared Street
(Private Street)

STREETSCAPES

Public vs Private



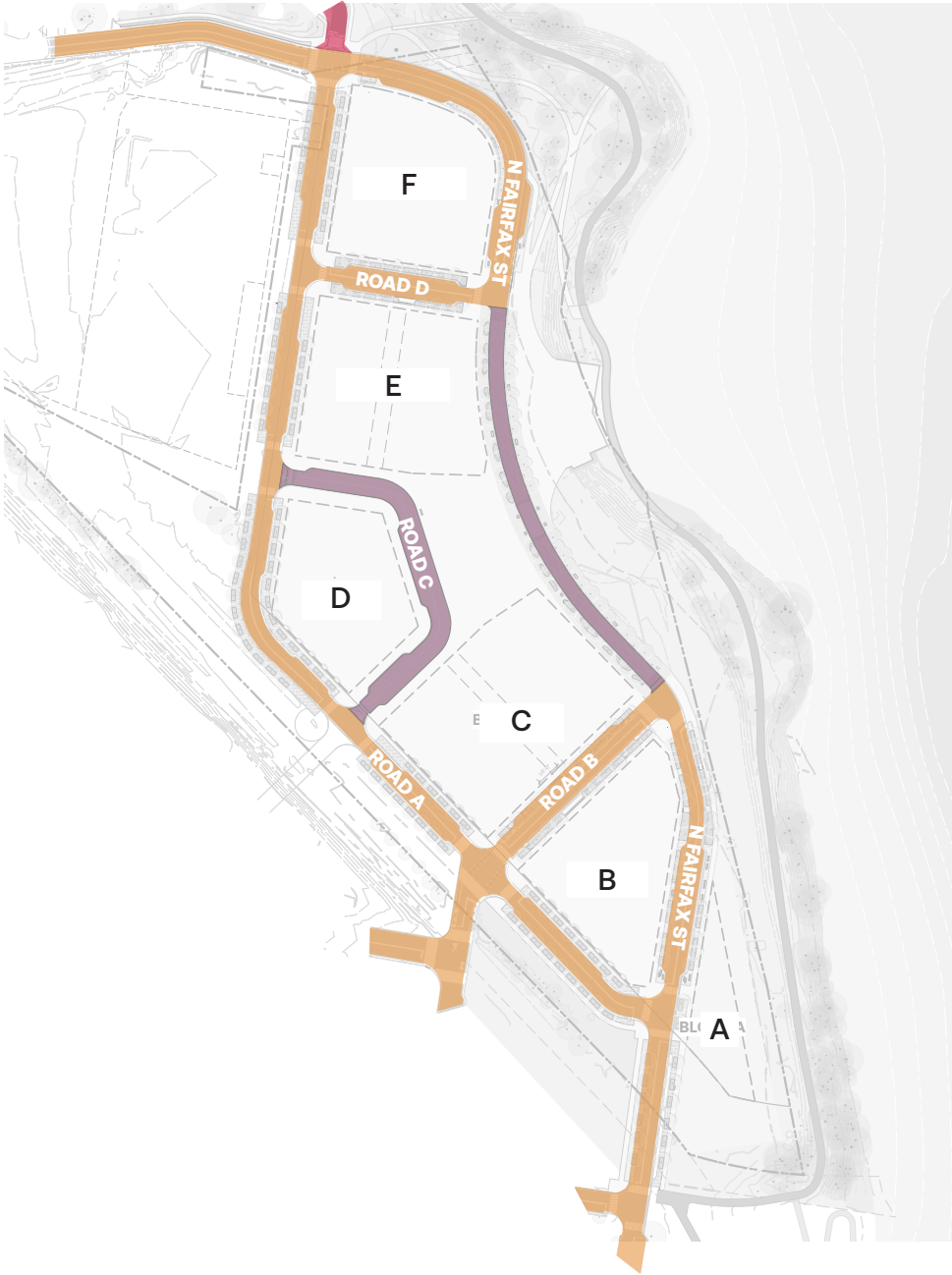
PUBLIC STREET



PRIVATE STREET

STREETSCAPES

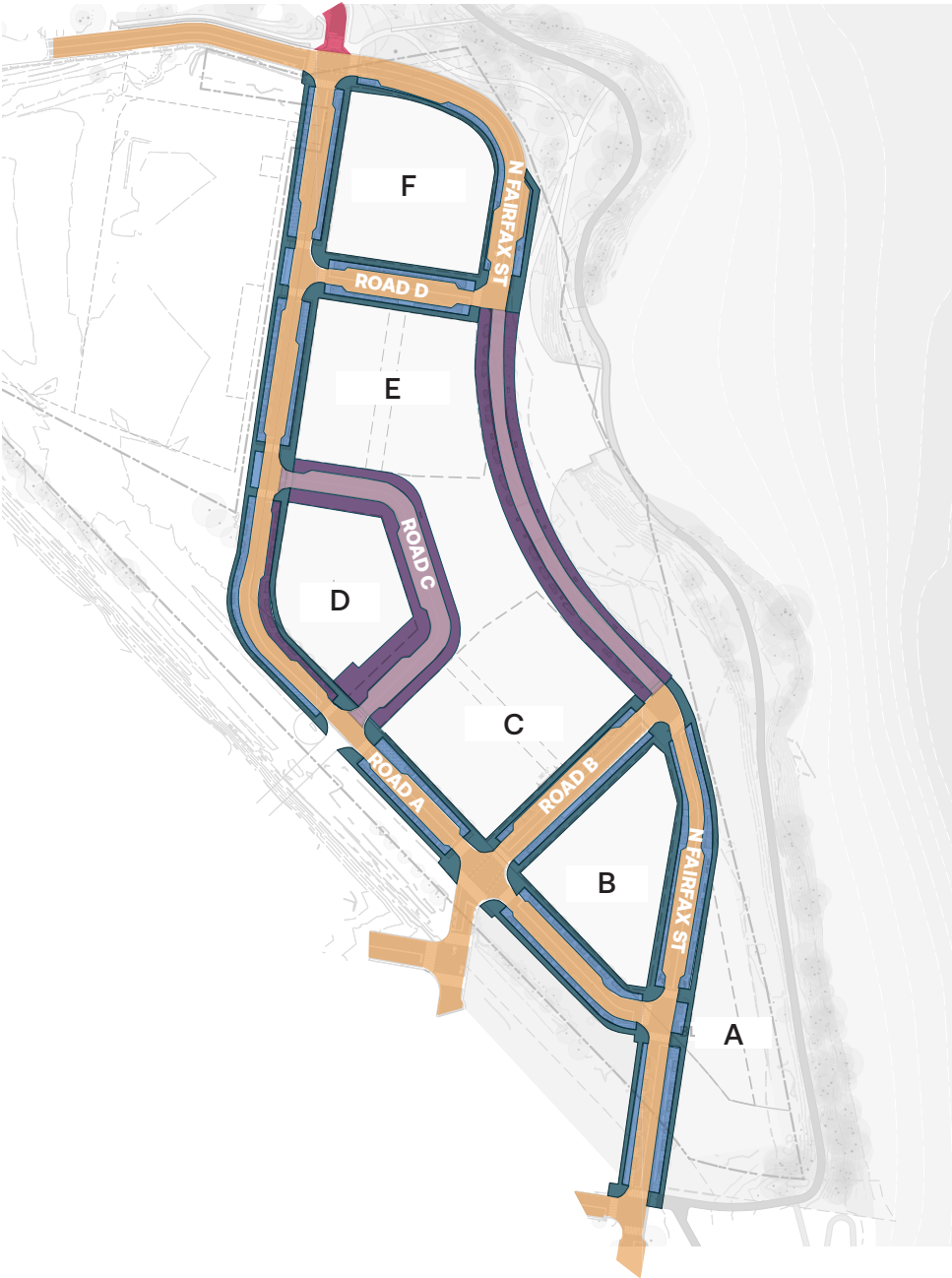
Roadway



- CONCRETE
- ASPHALT
- UNIT PAVERS

STREETSCAPES

Public Realm



Roadway

CONCRETE

ASPHALT

UNIT PAVERS

Public Realm

PEDESTRIAN ZONE

AMENITY ZONE

UNIT PAVERS (PEDESTRIAN-RATED)

GENERAL STREETSCAPES

Paving Materials



GENERAL STREETSCAPE:

- ROAD A
- ROAD B
- ROAD D
- N FAIRFAX ST EXTENSIONS

PEDESTRIAN
ZONE

CIP CONCRETE



AMENITY
ZONE

CIP CONCRETE OR UNIT PAVERS
(PEDESTRIAN RATED)

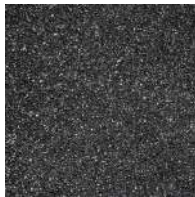


OR



ROADWAY

ASPHALT



*NOTE: FINAL DESIGN TO BE COORDINATED AND FURTHER REFINED WITH SUBSEQUENT BLOCK & OPEN SPACE DSUPS

Special Streetscapes

SPECIAL STREETSCAPES

Paving Materials



SPECIAL STREETSCAPE:

- WOONERF
- ROAD C

PEDESTRIAN/ AMENITY ZONE

UNIT PAVERS
(PEDESTRIAN RATED)



CURB

ROADWAY DELINEATION



SHARED ROADWAY

UNIT PAVERS
(VEHICULAR RATED)



CROSSWALK

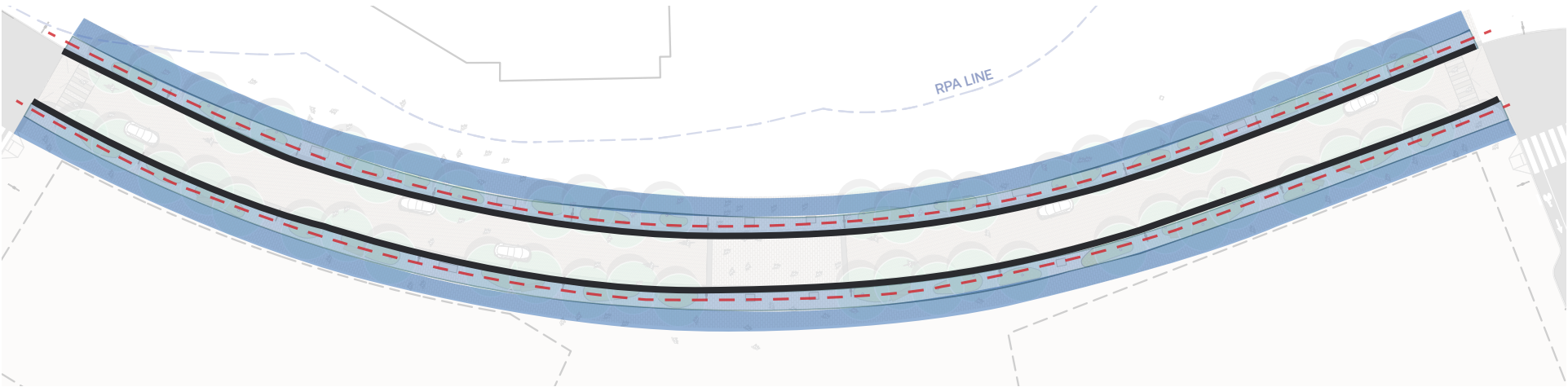
UNIT PAVERS
(VEHICULAR RATED)



*NOTE: FINAL DESIGN TO BE COORDINATED AND FURTHER REFINED WITH SUBSEQUENT BLOCK & OPEN SPACE DSUPS

WOONERF

Zones

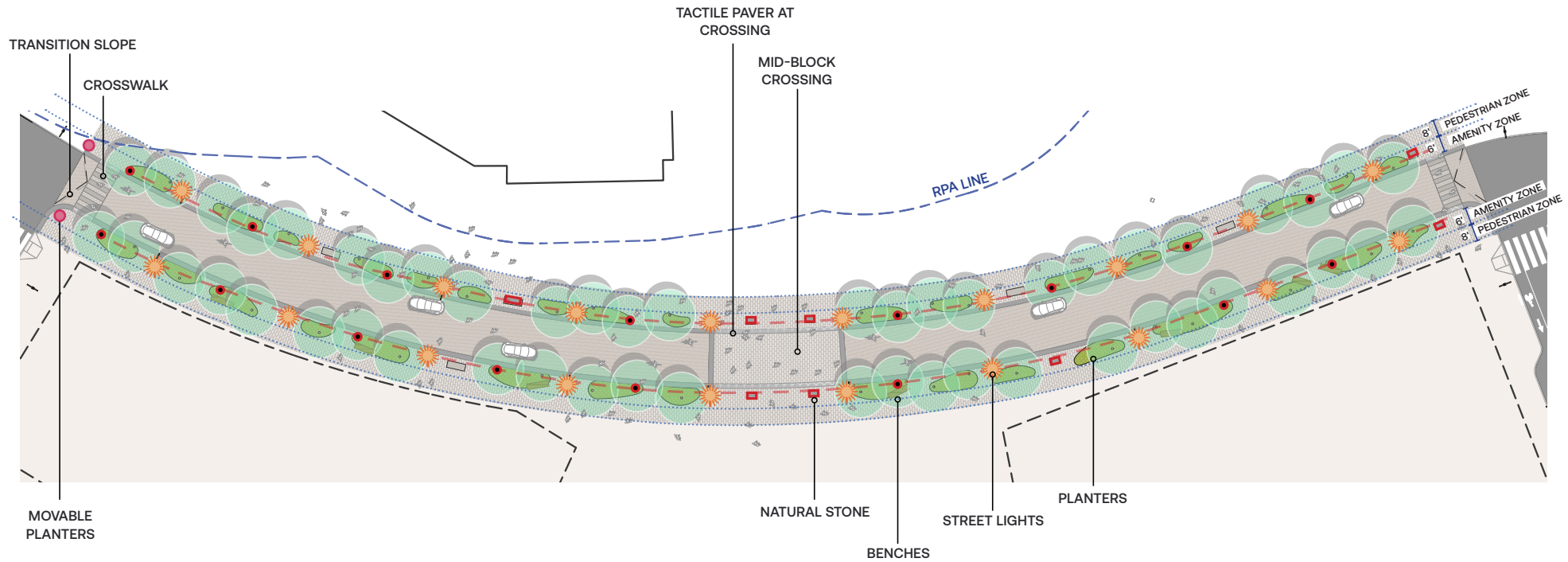


- ROADWAY DELINEATION
- 6' AMENITY ZONE
- 8' PEDESTRIAN ZONE
- VEHICULAR DETERANT LINE
(MAX 4' FROM FACE OF CURB)

*NOTE: FINAL DESIGN TO BE COORDINATED AND FURTHER REFINED WITH SUBSEQUENT BLOCK & OPEN SPACE DSUPS

WOONERF

Concept Plan



TREES (16' - 20' O.C.)



GROUND PLANTING



STREET LIGHTS (~60 O.C.)



NATURAL STONE



BENCHES



MOVABLE PLANTERS



*NOTE: FINAL DESIGN TO BE COORDINATED AND FURTHER REFINED WITH SUBSEQUENT BLOCK & OPEN SPACE DSUPS

Site Furnishings

CITY STANDARD SITE FURNISHINGS



ROADWAY LIGHTING

Preliminary Layout



STREET LIGHTING TYPE:

 ROADWAY LIGHTS (20' HT)

 PEDESTRIAN LIGHTS (16' HT)

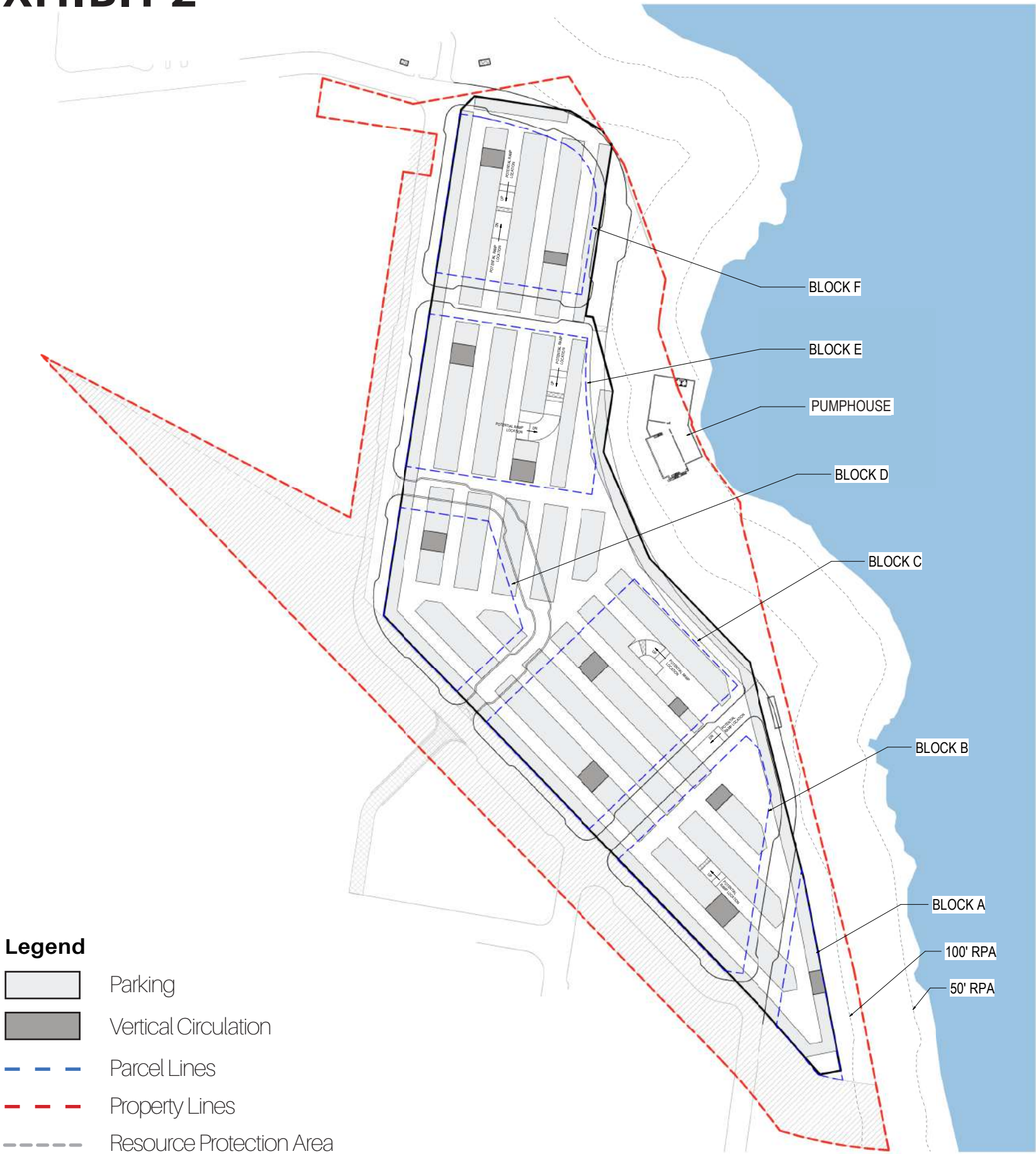


GARAGE EXHIBIT

LEVEL P1

Parking layout shown is conceptual and will vary, along with number of garage levels, based on the future land use program determined and submitted with each block DSUP to include future design considerations including vertical circulation, MEP systems, and structural grid.

EXHIBIT 2



SITE PV EXHIBIT

PV shown are approximate in size and location and may vary based on the design of the applicable open spaces that will be refined in further development of the Open Space DSUPS

EXHIBIT 3

